



Invitation to Submit Expression of Interest (EOI) for Direct Access to Mumbai Metro Line 3 Stations

Mumbai Metro Rail Corporation Limited
(A SPV JV of Govt. of India and Govt. of Maharashtra)

Office of Executive Director (Planning), NaMTTRI Building, E-Block, Plot No. R-13,

Bandra-Kurla Complex, Bandra (E), Mumbai - 400 051.

Website: <https://www.mmrcl.com>

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The objective of this document is to provide information to the interested parties about requirement of Mumbai Metro Rail Corporation Limited to facilitate their Application for the project. This document does not claim to contain all the information as may be required at the time of execution of work. Each applicant is advised to conduct its own survey for commissioning the work and check the accuracy, reliability and completeness of the information in this EOI document and obtain independent advice from appropriate sources as deemed necessary. Mumbai Metro Rail Corporation Limited makes no representation or warranty and shall incur no liability under any law, statute, rules or regulations as to the accuracy, reliability or completeness of this document. Mumbai Metro Rail Corporation Limited may, at their absolute discretion, but without being under any obligation to do so, update, amend or supplement the information in this document.

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MUMBAI METRO RAIL CORPORATION LIMITED

(A JV- SPV of Govt. of India and Govt. of Maharashtra)
NaMTTRI Building, Plot No. R-13, E-Block Bandra-Kurla Complex, Bandra (E),
Mumbai- 400 051

EXPRESSION OF INTEREST

Mumbai Metro Rail Corporation Limited, (MMRCL) invites Expression of Interest (EOI) from interested agencies for the work of

“Direct Access to Metro Line 3 Stations”

The developments including Commercial and Residential buildings located in proximity to the Metro Line 3 stations that desire to have a direct access to the Metro Line 3 stations should submit Expression of Interest in hard copy to the Executive Director (Planning), MMRC on the above address on or before Friday, 20/12/2019 by 06.00 pm.

Website: <https://www.mmrc.com>

Date: 01/11/2019

- Sd -

Place: Mumbai

Executive Director (Planning)

Metro Line 3 Stations - Direct Access EOI

1 About MMRCL

Mumbai Metro Rail Corporation Ltd (MMRCL), a JV-SPV (50:50) of Government of India (GoI) and Government of Maharashtra (GoM), is implementing Mumbai Metro Line 3 which is a 33.5 km fully underground metro corridor with 27 stations. The Metro Line 3 forms an integral part of the Mumbai Metro Master Plan as it provides unprecedented connectivity to Mumbai's business districts as well as major transport hubs in the city.

1.1 Alignment and Connectivity

The southern end of the alignment is at Cuffe Parade, a short distance from the World Trade Centre and Back Bay Bus Depot. The metro follows a northwards trajectory with stations at Vidhan Bhavan and Churchgate which serve the Nariman Point Business District as well as the Mantralaya and other Government offices. From Churchgate the metro heads north-east with stations at DN Road and on Mahapalika Marg which serve the Fort district, Chhatrapati Shivaji Maharaja Terminus (CSMT), Sessions Courts and the MCGM Headquarters. The alignment follows JSS Road serving the Kalbadevi and Girgaon areas and continues northwards to Grant Road, Mumbai Central and Mahalaxmi. The alignment follows E'Moses Road with stations at Science Museum, Worli Naka and Worli which shall serve the rapidly developing Lower Parel and Worli districts. The alignment continues northwards through Dadar, Mahim prior to turning eastwards to Dharavi before reaching Bandra Kurla Complex (BKC). The BKC station is strategically located so as to serve the G and F Blocks of the business district. The alignment continues northwards along the Western Express Highway before entering the Airport area where three stations are being constructed that shall serve the airport terminals and other developments in the airport area. The alignment further moves along MIDC Central Road abutting the entry to SEEPZ before terminating at Aarey Depot.

Metro Line 3 provides connectivity to:

- ~ 30 employment clusters; government and private offices
- ~ 12+ education institutions - schools, colleges & university
- ~ 11 major hospitals
- ~ 10 major transportation hubs
- ~ 25 religious & recreations areas
- ~ 3 million population in influence zone

1.2 Salient Features

- Train capacity 3,000 passengers (8 cars @ 8 passengers per square metre), designed for peak hour peak direction traffic (PHPDT) of 72,000 passengers
- Fully air conditioned
- Platform screen doors for passenger safety and energy efficiency
- Communication Based Train Control (CBTC) signalling system leading to driverless train operations
- Two-level stations provided with escalators & lifts, designed as barrier free access for physically challenged
- Automatic Fare Collection (AFC) system that can lead to integrated ticketing
- Maximum train speed of 85 kmph; journey speed of ~35 kmph

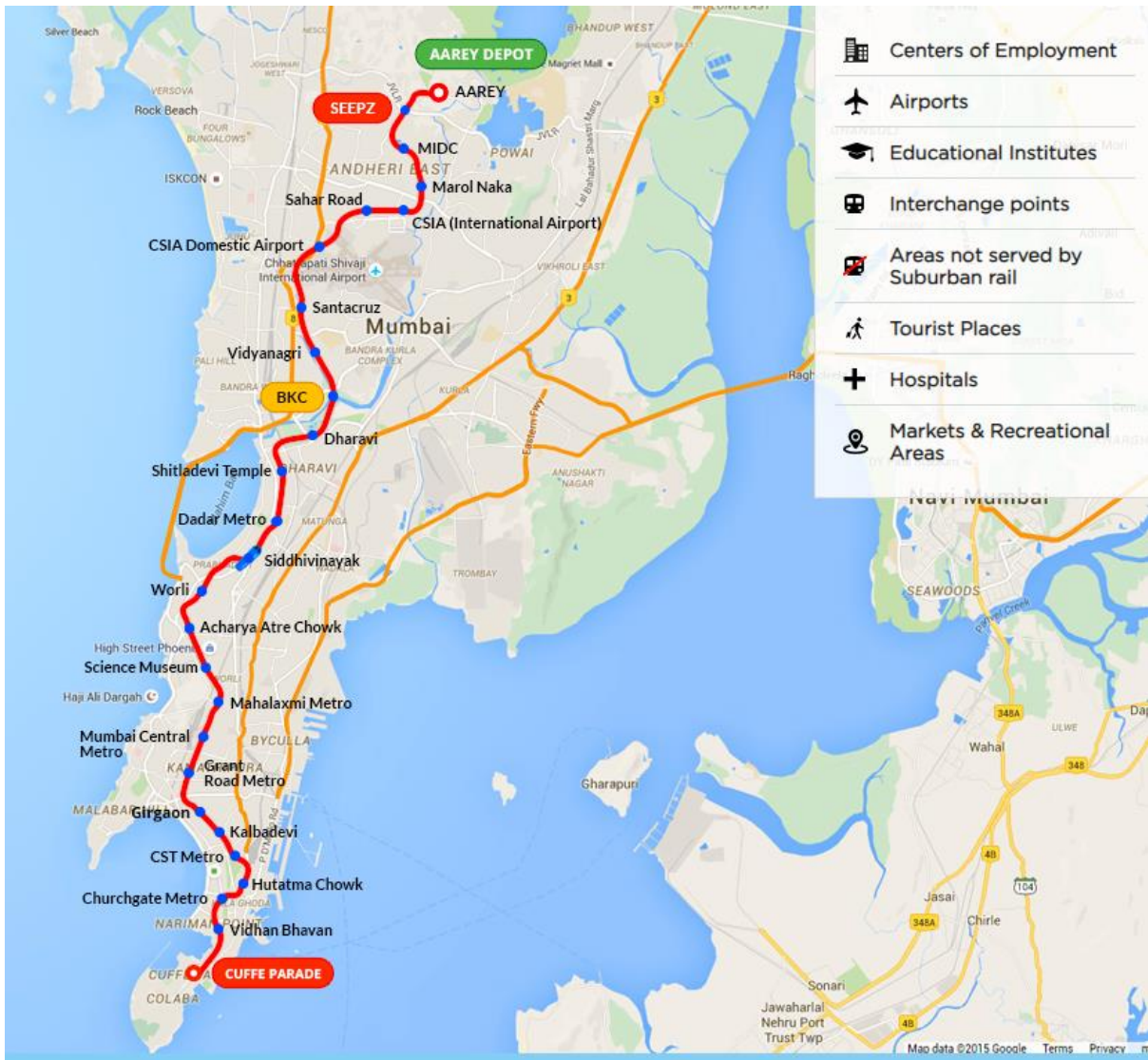


Figure 1: Metro Line 3 Alignment

1.3 Interchanges

The Line 3 also provides rapid connectivity to major transport hubs and transport stations in the city including the CSMT, Churchgate, Mumbai Central (Suburban and Long Distance) trains stations. Interchanges with the Metro Line 1 at Marol Naka, Metro Line 2B at BKC, Metro Line 7 & 8 at Airport

Terminal 2 and Monorail at Jacob Circle are planned. These interchanges will allow for unparalleled rapid access to all parts of the city.



Figure 2: Metro Line 3 Interchanges

1.4 Objective

Several metro stations are located in close proximity of major residential and commercial developments. Considering the metro station design, there is an opportunity for developing a direct access from the unpaid concourse area of the metro station to the adjoining developments. The direct access would comprise of an underground subway from the unpaid area at the concourse level to the adjoining development. The direct access connection would provide unhindered access from within the development to the metro station. This shall ensure safe and comfortable access to commuters avoiding conflicts with vehicular traffic and the vagaries of weather.

Typical details of the New Austrian Tunnelling Method (NATM) Tunnel are as follows:

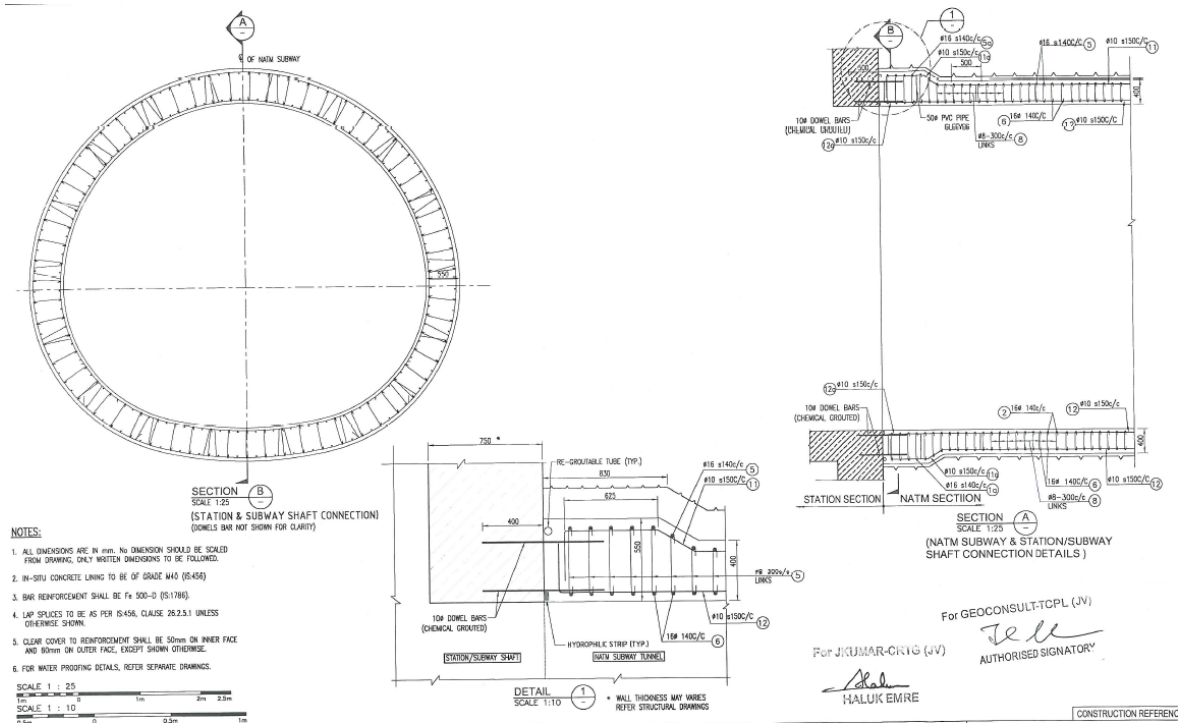


Figure 3: Typical Details for a NATM tunnel

MMRCL invites proposals from developments in the vicinity of the Metro Line 3 stations to partner with MMRCL to develop this direct access infrastructure.

2 Direct Access to Metro Stations

2.1 Station Location:

Majority of the metro stations are planned under the vehicular Right-Of-Way (ROW) with a few stations located just off the road under open spaces. The exact location of the stations can be found on the MMRCL website (<https://www.mmrcl.com/en/latest-tenders/37>).

The underground station shall be accessed via escalators, stairs and elevators that shall be located on the footpath or open areas in the proximity of the metro station. These entry/exit shall be designed as per universal accessibility standards and shall serve as the primary access for commuters to the metro station.

2.2 Station Design:

The metro stations are typically designed over two underground levels - the concourse and platform. The concourse level is typically located 10-14 metre below ground level and shall serve as the place where commuters shall arrive after taking the escalator / elevator or stairs from the ground level. The concourse level is split into two parts - an unpaid area and a paid area. The unpaid area is where the commuters shall land and is where commuters can purchase tickets from ticket vending windows and ticket vending machines. The unpaid area is separated from the paid area located at the centre of the station box by the security gates and fare collection barriers. The paid area accommodates vertical circulation elements (lifts, escalators and stairs) which allow for connectivity with the platform level.

The platform located typically 18-20 metre below ground level is typically 185-metre long and 12-metre wide and is planned as an island with the tracks located on either side of it. Along the platform are located platform screen doors which screen the platform from the train and ensure high levels of safety for commuters and reduces the likelihood of accidents.

Two stations, Cuffe Parade and Siddhivinayak have been designed as a three-level station with a mezzanine level located between the concourse and ground level. This level is envisioned to be leased out as a commercial space that could be used for retail / organised commercial space / office space / banks / educational institutes / restaurants etc. Other stations are planned such that the unpaid concourse area shall have travel commuter related retail spaces.

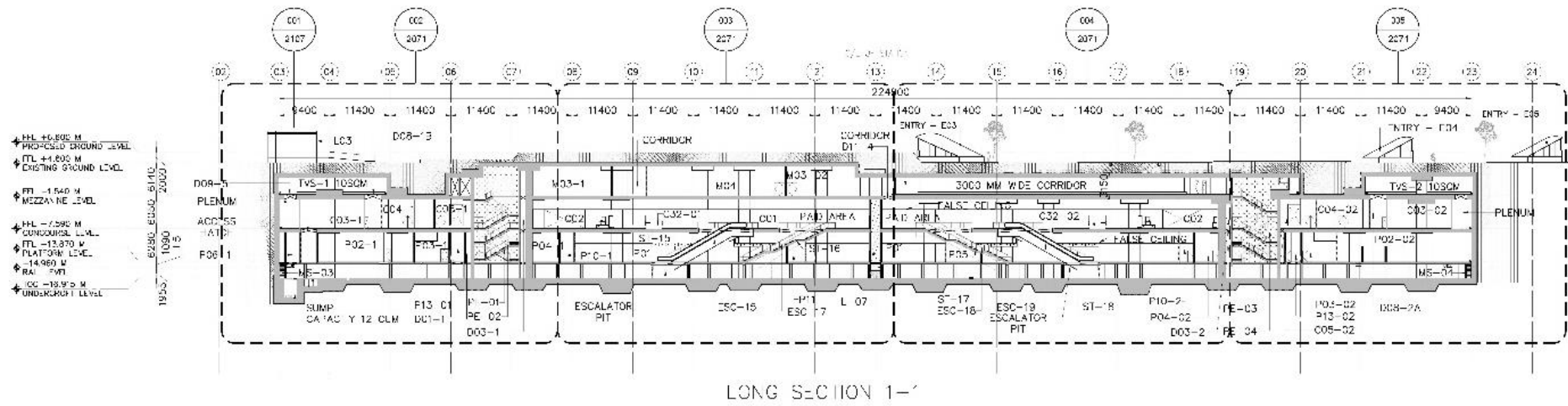


Figure 4: Typical Longitudinal Section of an Underground Station

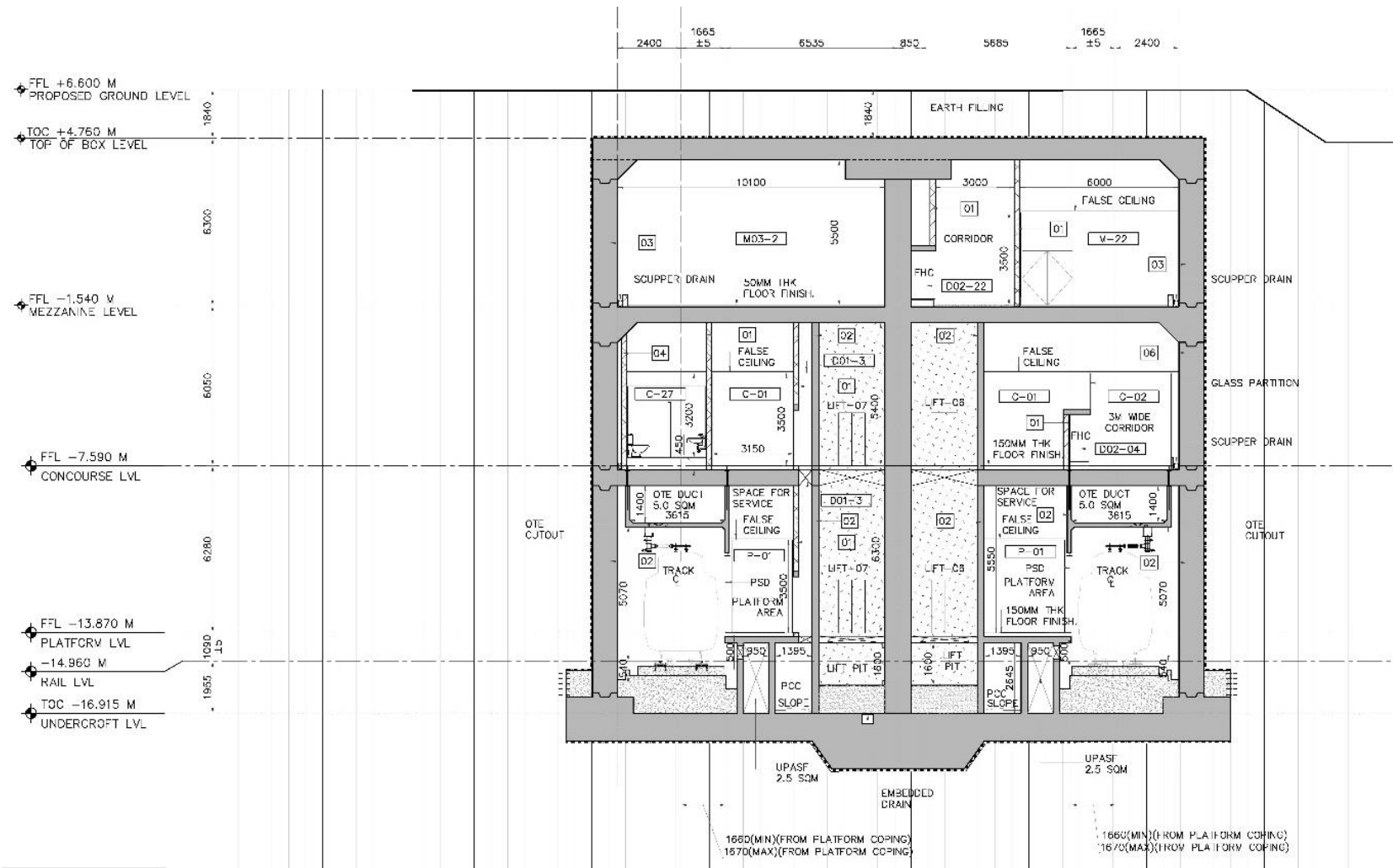


Figure 5: Cross Section of a Typical Underground Station

2.3 Station Security Features

The following features are planned for Line 3 –

SAFETY

- Platform Screen Doors to prevent accidents and entry to tunnels
- Personnel and baggage checks
- Full CCTV coverage
- Continuous surveillance 24X7
- Safe illumination
- All stations will have first aid facilities
- Hot line to hospitals from each station
- Staff trained for disaster management

POWER

- No power failure
- Fed by three different grids that ensure adequate redundancy
- Lights, lifts, escalators, fire systems to have two-level power back up
- Effective signage visible in low light and darkness

FIRE

- Station and tunnel ventilation system
- Separate fire lifts
- Sprinklers and fire alarms
- Fire rated power cables
- SCADA system that detects fire early and accurately
- Auto quenching fire prevention system

FLOODS

- Water-tight tunnel and station construction
- Station entry designed 1m above high flood level
- Sensor based pumps that automatically activate in emergency

CONNECTIVITY

- Tie-up with network service providers
- Wi-Fi enabled tunnels, stations & passenger areas
- Passenger services related apps

3 How to get Direct Access - Terms & Conditions

As MMRCL is in the process of implementation of the Metro Line 3 stations and considering the current status of civil works, it is well positioned to take up the development of the direct access subways.

The costs associated with the civil works, systems and electrical works, operations and maintenance (O&M) of the subways shall be borne by the Applicant (the “Developer”) seeking to get direct access to the metro station and the implementation / construction of the direct access subways shall be undertaken by MMRCL.

3.1 Process for Development of Direct Access

- i. MMRCL is issuing this Invitation for an EOI for developing direct access connections with the Metro Line 3 stations.
- ii. Developers who have properties in the vicinity of the Metro Line 3 stations shall respond with their EOI submission.
- iii. Jointly with MMRCL Projects and Planning team, alignment for the proposed direct access subway shall be agreed upon.
- iv. Based on the agreed alignment, a Detailed Project Report (DPR) and Bill of Quantities (BOQ) for the development of the Direct Access shall be created by MMRCL. No limits are set in terms of length and dimensions of the connection. The alignment and BOQ shall be finalised based on site conditions and remarks received from the area planning authority. For each direct access proposal, a separate DPR shall be prepared.
- v. It is proposed that the cost shall be equally shared amongst all the parties including MMRC. For example, in case there are two Developers sharing a common access, the cost for each party shall be 33.33%.
- vi. A Joint Development Agreement (JDA) shall be signed between the Developer and MMRCL that shall specify the project costs and other details including dimensions of the subway, specific HVAC requirements, finishes etc. Clear demarcation of ownership of the subway between MMRCL and the Developer will be included in the JDA.
- vii. MMRCL shall follow standard procurement processes and appoint a Contractor (the “Contractor”) for the said works, which shall be jointly supervised by the Developer and MMRCL.
- viii. A Construction Agreement for the civil and systems works shall be signed between the Contractor and MMRCL that shall specify the construction specifications, construction schedule, payment schedule, etc.
- ix. Any additional connections and changes in alignment in the future will require express written approval of MMRCL.
- x. In case there is a change of ownership of the Developer entity, a Deed of Accession will have to be signed between the outgoing and the new owners so that continuity of the rights and obligations is ensured.

3.2 Rights and Obligations

- i. The subway ownership under public roads and public areas shall remain with MMRCL and the ownership of the sections of the subway within the Developers' property shall rest with the Developer. Requisite building permissions will have to be sought by the Developer including NOCs from MMRC for such subway.
- ii. The 'Hard O&M' works including electrical, HVAC and Systems shall be carried out by MMRCL. An independent electrical meter shall be installed which shall measure the power consumption of the dedicated access subway.
- iii. 'Soft O&M' works such as housekeeping shall be taken up directly by the Developer.
- iv. Branding rights within the dedicated access subway shall rest with the Developer. The advertisement should be in adherence to applicable regulations. There shall be no third-party advertising permitted.
- v. No retail or vending activity shall be permitted within the dedicated access subway, without prior express written consent by MMRCL.
- vi. The Developer may have kiosks which may serve as concierge and meet and greet desks. The design and location of these kiosks would have to be approved by MMRCL.
- vii. The Developer may include security desks/kiosks within the part of the subway that is under the ownership of the Developer.
- viii. The design, dimensions and finish in terms of colour scheme, lighting and features can be proposed by the Developer; however, the final designs shall be approved by MMRCL.
- ix. The operation hours of the direct access subway shall be mutually agreed upon between MMRCL and the Developer; where the Developer shall try to match the operating hours of the direct access to the operating hours of the metro to the extent feasible.
- x. All regulatory permissions, approvals, etc. and its renewal will remain the responsibility of MMRCL.

3.3 Costs

The cost of construction and O&M is dependent on the site conditions, proposed design and construction methodology. These engineering details and the costs shall be decided at the DPR stage. However, based on the current cost of construction, the following estimates have been arrived at –

- Civil works cost for the subway (envisaged to be 6m wide but will be determined jointly) employing New Austrian Tunnelling Method (NATM) is estimated at Rs. 10,00,000/- per running metre
- Electrical and systems cost is estimated at Rs 2,00,000 / running metre
- Annual O&M cost is estimated at 5% of the total cost of construction, electrical and systems works
- These costs are purely indicative and shall be determined based on DPR and on actuals.

It is envisaged that a common direct access (for part of the subway) may be provided to multiple properties located at the same side of the metro station to optimise costs. In such cases the following guidelines will apply –

- Common subway to the extent feasible; thereafter separate subways branching out to the individual properties

- The common subway alignment will be jointly discussed and agreed with all interested Developers
- Costs of the common portion of the subway will be borne equally by all Developers
- For illustration – subway length for Developer A = 30 metre, Developer B=40 metre, Developer C=50 metre, common portion of the subway =20 metre, then costs related to 20 metre of the common subway will be split equally between the three Developers

In addition to the above costs, MMRCL shall charge a one-time access fee of Rs 2 Crores (Rupees Two Crores only) as premium for granting direct access to its metro stations. In case of common subway for multiple Developers, the access fee of Rs. 2 Crores shall be paid by each Developer and there shall be no sharing of Access Fee. These funds, along with other sources of finance, will be utilised by MMRCL to bridge the financing gap for the overall Line 3 public infrastructure project.

3.4 Payment Terms

3.4.1 Civil Works

- The civil works shall be issued and awarded by following applicable government norms.
- Payments to the Contractor shall have to be made as per the payment schedule which shall be jointly agreed by the MMRCL and the Developer based on milestones. The Developer shall make the agreed upon payments as per schedule specified in the Joint Development Agreement (JDA) to MMRCL and consequently MMRCL will release the payments to the Contractor.
- A separate escrow bank account may be set up by MMRCL to facilitate the movement of funds.
- The Developer shall furnish an irrevocable Bank Guarantee (BG) to MMRCL equivalent to the entire EPC contract cost at the time of awarding the contract.
- Additionally, the Contractor shall furnish an irrevocable Performance Bank Guarantee (PBG) to MMRCL equivalent to the entire EPC contract cost at the time of awarding the contract.
- MMRCL will call for payments to be made by the Developer as per the agreed payment schedule. Payment notice will be furnished by MMRCL to the Developer two weeks in advance of the agreed payment dates. In the event that the Developer fails to make the said payment, MMRCL shall enforce the Bank Guarantee to make the said payments to the Contractor.

3.4.2 Access Fee

- Payment of Access Fee shall be made in 3 instalments as given hereunder –
 - 1/3rd at the time of signing the Joint Development Agreement
 - 1/3rd at the time of awarding the works contracts
 - 1/3rd at the time of completion of construction of the direct access subway
- The Developer shall furnish an irrevocable Bank Guarantee at the time of the JDA equivalent to the residual 2/3rd amount of the Access Fee. Thereafter the bank guarantee shall be adjusted to the residual amount payable after award of the works contract.

- MMRCL will call for payments to be made by the Developer as per the above schedule. Payment notice will be furnished by MMRCL to the Developer two weeks in advance. In the event that the Developer fails to make the said payment, MMRCL shall enforce the Bank Guarantee to recover monies due against the Access Fee.

3.4.3 O&M Cost

- MMRCL shall invoice the Developer on a quarterly basis on actuals.
- The Developer shall furnish an irrevocable Bank Guarantee equivalent of four quarters of projected O&M costs.
- In case multiple Developers are sharing a common subway the O&M cost will be borne by the Developers proportionately as explained in 3.3 (illustration) above.
- In the event that the Developer fails to make the said payment, MMRCL shall enforce the Bank Guarantee to recover monies due towards O&M costs.
- Additionally, MMRCL will have the right to block the entrance from the Developers' end of the subway. However, the public entrance from the public street/ footpath outside the Developers' premises will continue to be operational. In case, such public access point from the footpath has not been constructed, MMRCL will have the right to construct such public access point at its own cost.

4 Expression of Interest Submission Requirements

4.1 Expression of Interest Letter

(To be submitted with EOI Application by Applicant on Company Letter Head)

To
Executive Director (Planning)
Mumbai Metro Rail Corporation Limited
NaMTTRI Building, E-Block, Plot No. R-13,
Bandra-Kurla Complex, Bandra East, Mumbai 400051

Subject: Direct Access to Mumbai Metro Line 3 Station

Reference: Notice for EOI No. _____ dated_____.

Sir,

With reference to the above invitation for Expression of Interest (EOI), we have examined and understood the instructions, terms and conditions provided in Invitation for EOI. We hereby enclose our EOI Application in the prescribed format as mentioned in Invitation for EOI.

We confirm that we agree with the instructions, terms and conditions provided in Invitation for EOI. The undersigned declares that the statements made, and the information provided in the duly completed application are complete, true, and correct in very detail. *(Name of Applicant)* is interested in partnering with Mumbai Metro Rail Corporation Limited for developing Direct Access connections from our development to the *(name of station)* Mumbai Metro Rail Station.

We also understand that Mumbai Metro Rail Corporation Limited is not bound to accept the offer either in part or in full. If the Corporation rejects the offer in part or in full, it may do so without assigning any reasons thereof.

Yours Faithfully,

Authorised Signatory

(Name, Designation & Seal of the firm)

Date: _____

Place: _____

4.2 Company Details

The following details are required to be submitted for the company seeking direct access to a Line 3 metro station -

1. General information of the company
2. Core business areas of operation, number of years in the business, ownership and organisational structure of the Company
3. Audited financial statements for the last year showing financial capability, of F.Y. 2018-19 and half-year 2019-20
4. Development agreement for the property from where direct access is proposed
5. Ownership document for the property from where direct access is proposed

If a Special Purpose Vehicle (SPV) has been created for the development of the property in the vicinity of the metro station, then the above details have to be provided for the parent holding company as well as the SPV. If the plot is being developed by a consortium, the above details have to be provided for all members of the consortium.

4.3 Proposed Alignment

The station locations including the station box can be found on the MMRCL website at (www.mmrcl.com). Considering the location of your property and the development planned within it, a preferred alignment connecting your property to the unpaid section of the Metro Line 3 station is to be submitted in soft copy format. The alignment is to be marked on the satellite image and the overall length of the direct access subway is to be mentioned on the alignment map.

5 EOI Submission Details

5.1 Submission Details

The application form along with the accompanying documents should be submitted in a closed and sealed envelope duly super scribed with “Direct Access to Mumbai Metro Line 3 Stations” on or before Friday, 20 December, 2019 by 06.00 pm addressed to:

Executive Director (Planning), MMRCL
NaMTTRI Building, Plot No. R-13,
E-Block Bandra-Kurla Complex,
Bandra East, Mumbai 400051

Contact Person:

Mr. Swetal Kanwalu, AGM (Town Planning)

Phone: 022-26384693

5.2 Submission of Queries

If the applicant has any queries regarding the EOI, they must be sent in writing, or by standard electronic means, to all of the following email IDs -

1. swetal.kanwalu@mmrcl.com
2. shadab.siddiqui@auctusadvisors.in
3. rahul@studiopod.in

MMRCL will respond in writing, or by standard electronic means to all applicants. If MMRCL deems it necessary to amend the EOI as a result of a clarification, it shall do so following the procedure described below:

- At any time before the EOI submission deadline, MMRCL may amend the EOI by issuing an amendment in writing or by standard electronic means. The amendment shall be uploaded on the MMRCL website and will be binding on all interested parties.
- If the amendment is substantial, MMRCL may extend the EOI submission deadline to give the applicants reasonable time to take the amendment into account in their EOI.

6 Tentative Schedule for Direct Access

The following schedule is proposed by MMRCL for the process of securing direct access -

SN	Particulars	Timeline
1	Publishing of EOI	Friday, 1 November, 2019
2	Submission of queries by Developers	Friday, 22 November 2019
3	Response to queries by MMRCL	Friday, 6 December 2019
3	Submission of EOI	Friday, 20 December 2019
4	Formal discussions regarding direct access	December 2019 – January 2020
5	Signing of JDA	February 2020
6	Preparation of DPR	February 2020 – April 2020
7	Publishing of tender for civil works	May 2020
8	Award of civil works	June / July 2020

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